




Los Angeles County
Metropolitan
Transportation
Authority

818 West Seventh Street
Suite 300
Los Angeles, CA 90017

213.623.1194

September 15, 1995

TO: MTA BOARD OF DIRECTORS
FROM: STANLEY G. PHERNAMBUCQ 
SUBJECT: MTA CONSTRUCTION NONCONFORMANCE REPORTS

Attached please find copies of the Nonconformance Reports (NCRs) received by the MTA Director of Quality Management for the week of September 1 - 14, 1995.

If you have any questions, please call Garry Warren, Acting Director of Quality Management, at 213-244-6216.

SGP:GW:gg (E-MEMOSFOR CIVR.5)

Attachments

cc: John Adams
Tom Carmichael
Gwen Williams
Charles Stark
Dave Sievers
Elaine Stewart
Garry Warren
Kim Kimball
Judy Schwartz

RR 8/24/95

May Affect ROD		
<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	<input type="checkbox"/> NA
RR		8/24/95
Initials		Date

NONCONFORMANCE REPORT

Page 1 of 2

1. Line: Contract No: <i>ROS</i> C-6430		Year: 1995	NCR No.: 17	2. Contract Name: Arroyo Seco BR.		3. Date: 8-23-95	
4. Location: Bent # 6 & 7			5. NCR Type: Corrosion Protection		6. Contractor/Supplier: Nation wide		
7. Specification/Drawing No.: CP-103			8. Originator: Mohammad Alazzah				Phone Number: (213) 258-7284
9. Nonconformance description and contract requirements: Nation wide did not perform fillet welding on the pedestals for bents # 6 & 7 (see the attached sheet). The pedestals are already poured & backfilled by Kiewit.							
10. Resident Engineer: <i>Robert J. Ellis</i>		Date: 8/24/95	11. Reply requested from: JEFF ELLIS		12. Reply due date: 8/30/95		
13. Root cause of the problem (completed by contractor): STIRRUPS WERE NOT INSTALLED ON VERTICAL BARS AT THE TIME THE WELDER WAS ON SITE, AND THE WELDER FAILED TO NOTIFY NATIONWIDES FOREMAN.							
14. Corrective action(s) to prevent recurrence (completed by contractor): NATIONWIDE HAS BEEN NOTIFIED TO BRING THE WELDER OUT TOMORROW (8/25/95) TO PERFORM THIS WORK AT THE LAST LOCATION: TOWER 9/10.							
15. Prepared by: J. ELLIS		Date: 8-24-95	16. Implementation Date: 8/24/95		17. RE Approval: <i>Robert J. Ellis</i>		
18. Disposition:		<i>SEE COMMENTS, ATTACHED</i>					
<input type="checkbox"/> Reject		<input type="checkbox"/> Rework		<input checked="" type="checkbox"/> Repair		<input checked="" type="checkbox"/> Use-As-Is	
19. <i>(SEE COMMENTS ATTACHED)</i> Engineer Approval: <i>John French</i>		Print Name: JOHN FRENCH	Signature: Date: 9-8-95		20. Resident Engineer: Date:		
21. Verification that nonconforming condition has been corrected:							
22. Inspection:							
		Print Name		Signature		Date	

8/29/95

<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	C
<i>RR</i>		N
Initials		Date
		<i>8/24/95</i>

NONCONFORMANCE REPORT

Page 1 of 1

1. Line: Contract No: <i>ROS - C630 - 95'</i> Year: <i>18</i> NCR No.:		2. Contract Name: <i>ARROYO SECO BRIDGE RECON.</i>		3. Date: <i>8/24/95</i>	
4. Location: <i>ABUTMENT 1</i>		5. NCR Type: <i>BEARING ELEVATION</i>		6. Contractor/Supplier: <i>KIEWIT PACIFIC</i>	
7. Specification/Drawing No.: <i>S-115</i>		8. Originator: <i>ROB RICHARDSON</i>		Phone Number: <i>(213) 258-7284</i>	
9. Nonconformance description and contract requirements: <i>THE NORTH BEARING SEAT IS APPROXIMATELY 1" LOW AND THE SOUTH BEARING SEAT IS APPROXIMATELY 1/2" HIGH.</i>					
10. Resident Engineer: <i>[Signature]</i> Date: <i>8/24/95</i>		11. Reply requested from: <i>JEFF ELLIS</i>		12. Reply due date: <i>8/25/95</i>	
13. Root cause of the problem (completed by contractor): <i>SEE SERIAL LETTER NO. 90</i>					
14. Corrective action(s) to prevent recurrence (completed by contractor): <i>SEE SERIAL LETTER NO. 90</i>					
15. Prepared by: <i>J. Ellis</i> Date: <i>8-25-95</i>		16. Implementation Date: <i>8/29/95</i>		17. RE Approval: <i>[Signature]</i> Date: <i>8/25/95</i>	
18. Disposition: <input type="checkbox"/> Reject <input type="checkbox"/> Rework <input checked="" type="checkbox"/> Repair <input type="checkbox"/> Use-As-Is					
19. Engineer Approval: <i>N.P. Pandya</i> Print Name: <i>N.P. Pandya</i> Signature: <i>[Signature]</i> Date: <i>8/29/95</i>		20. Resident Engineer: _____ Date: _____			
21. Verification that nonconforming condition has been corrected:					
22. Inspection:					
		Print Name		Signature	
				Date	

KIEWIT PACIFIC CO.

A Kiewit Company

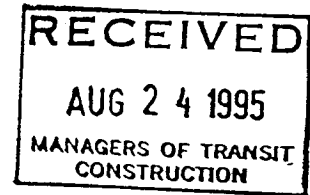
ARROYO SECO
P.O. Box 42248
309 Arroyo Seco Park Way
Los Angeles, CA 90042
213/550-8977
FAX 213/550-1092

PACIFIC STRUCTURES OFFICE
P.O. Box 1769
Vancouver, WA 98668-1769
360/693-1498
FAX 360/693-5582

August 23, 1995

(S.L. No. 90)

Managers of Transit Construction
311 Arroyo Seco Park Way
Los Angeles, CA 90042



Attention: Rich Hebert

Regarding: Abutment 1 Girder Seats

Gentlemen:

We are writing to notify your office of some defective work that we need to correct at abutment number 1. Two of the girder seats at that location shifted during concrete placement, and are not at the correct elevation. The north seat is approximately 1" low, the south seat is approximately 1/2" high, and the middle seat is good. The two seats that are wrong will have to be chipped out and reset at the correct elevation. This work, of course, will be at no cost to the authority.

The corrective work procedure will consist of the following:

- 1.) Sawcut a uniform rectangle 5" to 6" outside the girder seat, 1/2" deep.
- 2.) Chip out the girder seat maintaining uniform edges around the area of removal.
- 3.) Thoroughly clean the surrounding concrete and exposed reinforcing steel.
- 4.) Set and secure the girder seats to the correct line and grade.
- 5.) Dampen the surrounding concrete and pour back with non-shrink, non-metallic grout.
- 6.) Wet finish the grout to match the surrounding concrete.

If there are any questions regarding this matter, please contact the undersigned at our jobsite office.

Very Truly Yours,
Kiewit Pacific Co.

A handwritten signature in black ink, appearing to read "Jeffrey D. Ellis".

Jeffrey D. Ellis
Project Manager

cc: Parke Ball, KPC

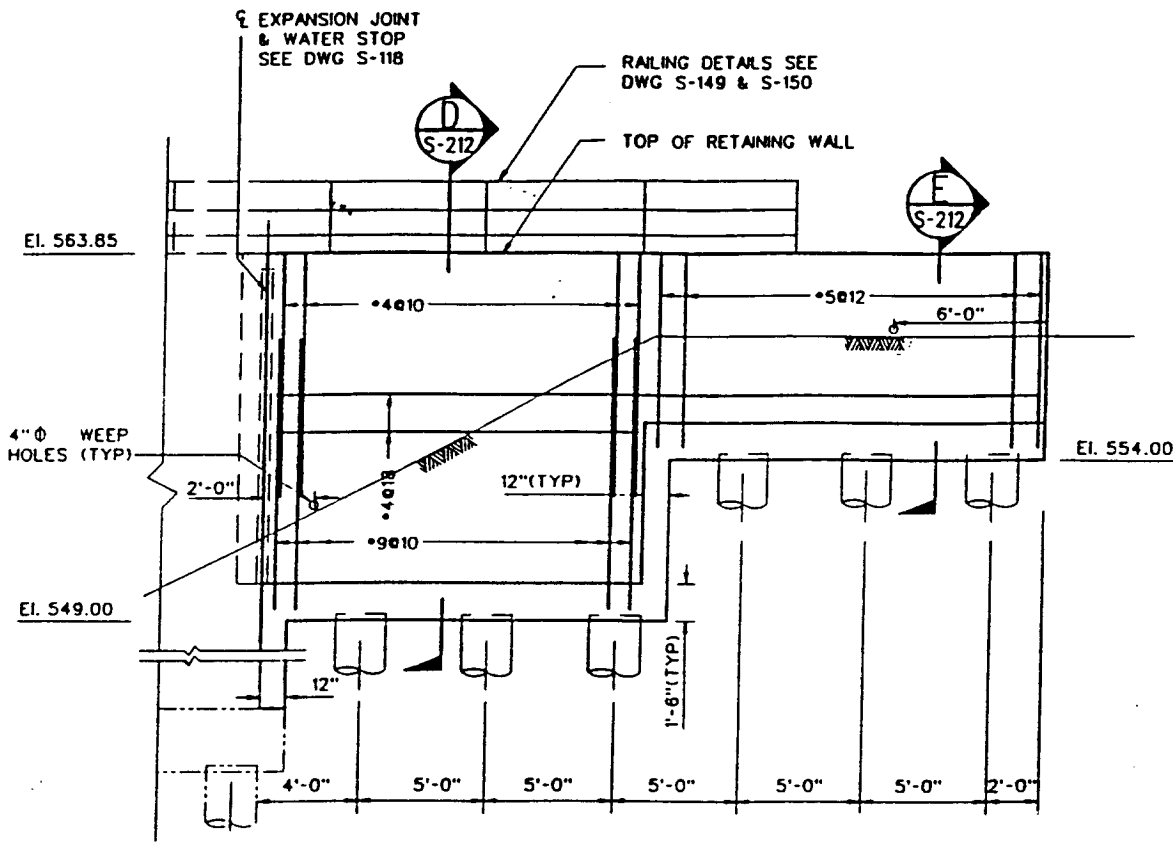
9/14/95

<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
<i>RA</i>	<i>8/28/95</i>
Initials	Date

NONCONFORMANCE REPORT

Page 1 of 1

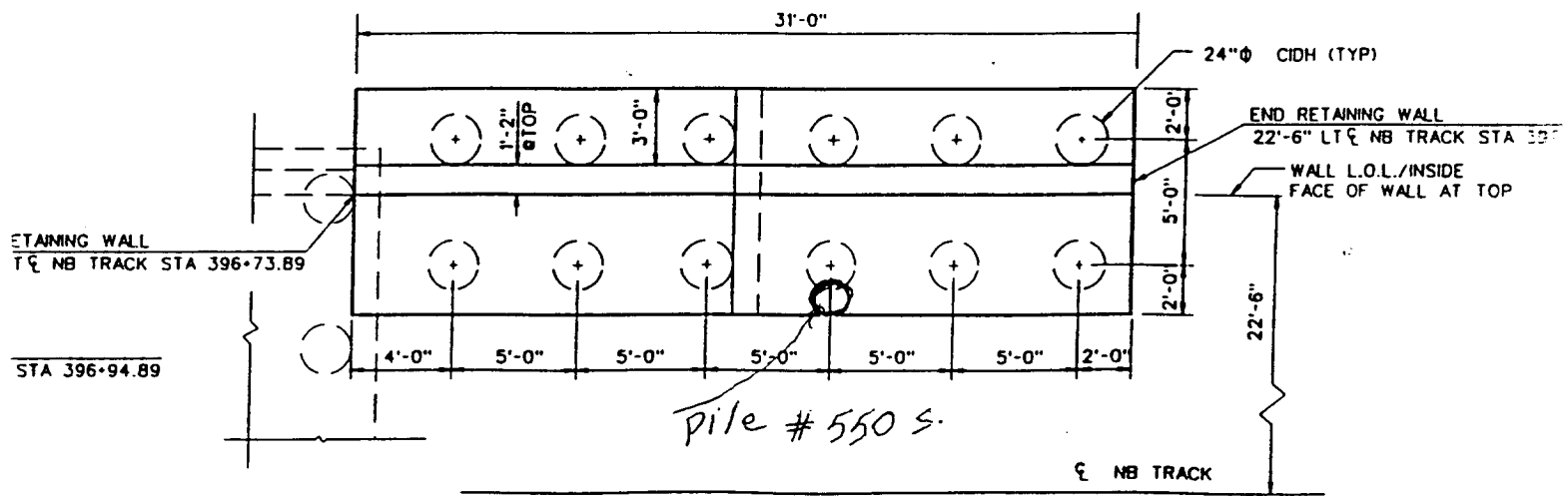
1. Line Contract No: Year: NCR No: <i>R-05 C-6430 1995 18 19</i>		2. Contract Name: <i>Arroyo Seco</i>		3. Date: <i>8-28-95</i>	
4. Location: <i>Abut. 12 Interface wall (North)</i>		5. NCR Type: <i>24" CIDH pile</i>		6. Contractor/Supplier: <i>Gence</i>	
7. Specification/Drawing No.: <i>2387-1.2E, plan sheet # 130</i>			8. Originator: Phone Number: <i>Mohammad Alazzah (213) 258 7284</i>		
9. Nonconformance description and contract requirements: <i>The 24" CIDH pile # 550 S. constructed under abut. 12 interface wall (North) had been shifted toward the edge of the footing by 10" to 12" from it's corrected location according to plan sheet # 130 (see the attached sheet # 2 of 2)</i>					
10. Resident Engineer: <i>ACTING</i> <i>Rahat J. [Signature]</i>		Date: <i>8/28/95</i>		11. Reply requested from: <i>JEFF ELLIS</i>	
				12. Reply due date: <i>8/29/95</i>	
13. Root cause of the problem (completed by contractor):					
14. Corrective action(s) to prevent recurrence (completed by contractor):					
15. Prepared by: Date:		16. Implementation Date:		17. RE Approval: Date:	
18. Disposition: <input type="checkbox"/> Reject <input type="checkbox"/> Rework <input type="checkbox"/> Repair <input type="checkbox"/> Use-As-Is					
19. Engineer Approval: Print Name: Signature: Date:			20. Resident Engineer: Date:		
21. Verification that nonconforming condition has been corrected:					
22. Inspection: Print Name Signature Date					



INTERPHASE	
WALL LOCATION	
396+73.89-396+89.89	
396+89.89-396+44.89	

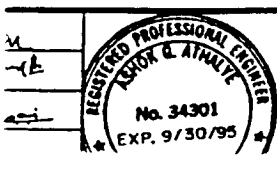
INTERPHASE	
WALL LOCATION	
396+73.89-396+89.89	
396+89.89-397+04.89	

ELEVATION - ABUT 12 INTERFACE RETAINING WALL (NORTH)



PLAN - ABUT 12 INTERFACE RETAINING WALL (NORTH)

ISSUED FOR CONSTRUCTION



Athalye Consulting Engineers
 23201 Mill Creek, Suite 240

Rail Construction Corporation
PASADENA BLUE LINE

SENIOR ENGINEERING CONSULTANT
Professional Engineer License No. 34301
 State of California
 Exp. 9/30/95

LAUPT TO PASADENA
ARROYO SECO BRIDGE
RETAINING WALL

9/8/95
Metropolitan Transportation Authority

bc: M. Latch - Area 2
 A. Hodge - Area 2
 C. Dixon - B251
 A. Yahontov - EMC
 D. Compton - MTA
 G. Warren - MTA
 R. Trepp - PD/QA
 T. Welch - RE B252

May Affect ROD		
<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	<input type="checkbox"/> NA
<i>[Signature]</i>		9/5/95
Initials		Date

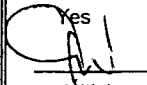

Nonconformance Report

1. Line: Contract No.: Year: NCR No.: MRL B252 95 048		2. Contract Name: VERMONT SANTA MONICA STATION & CROSSOVER		3. Date: 8/4/95	
4. Location MUD SLAB 411+40 TO 414+92		5. NCR Type: HDPE		6. Contractor/Supplier: KIEWIT	
7. Specification Section/Drawing No.: S-031, S-032		8. Originator: <i>[Signature]</i> Gary Rudy		Phone No.: 913-0095 Hold Tag No.: N/A	
9. Contract requirement and nonconformance description: CONTRACT DRAWINGS REQUIRES THE SLAB ON GRADE TO BE AT LEAST 4'-0" THICK - SURVEY OF THE HDPE PROTECTION SLAB FROM 411+40 TO 414+92, INDICATES THAT THE AS BUILT ELEVATION VARIES FROM PLUS (+) 1 3/4" TO MINUS (-) 1 3/4" FROM DESIGN ELEVATION - (SEE ATTACHMENT #1 & 2)					
10. Lead Inspector: <i>[Signature]</i> Gary Rudy		Date: 8/4/95		11. Reply requested from: S. LEE	
				12. Reply due date: 8/11/95	
Root cause of the problem and action(s) to prevent recurrence (completed by the contractor): WE WERE NOT CONTROLLING FINISHING OPERATION AS GOOD AS WE WANT DURING THE CONCRETE PLACEMENT OPERATION. AFTER WE IMPROVE OUR Q/C PROCEDURE, THE PROBLEM IS SOLVED.					
14. Corrective action(s) (completed by the contractor): WE REDRESSED THE TOP OF PROTECTION SLAB ACCORDING TO CONTRACT REQUIREMENT.					
15. Prepared By: <i>[Signature]</i> S. Lee		Date: 8-31-95		16. Implementation Date: 8-10-95	
				17. RE Approval: <i>[Signature]</i> T. Welch	
18. Disposition status: <input type="checkbox"/> Reject <input checked="" type="checkbox"/> Rework <input type="checkbox"/> Repair <input type="checkbox"/> Use-As-Is					
19. Engineer approval (Repair and Use-As-Is): N/A		Print Name: Signature:		Date: 20. Lead Inspector: <i>[Signature]</i> Gary Rudy	
21. Verification that nonconforming condition has been corrected: ALL AREAS THAT ARE ABOVE THE DESIGN ELEVATION HAVE HAD THE EXCESSIVE PROTECTION SLAB REMOVED & THE ELEVATIONS ARE NOW ACCEPTABLE - THE LOW AREAS WILL BE FILLED WITH STRUCTURAL CONCRETE - ACCEPTABLE					
22. Inspection: <i>[Signature]</i> GARY RUDY		Print Name:		<i>[Signature]</i> Gary Rudy	
				Date: 9/5/95	

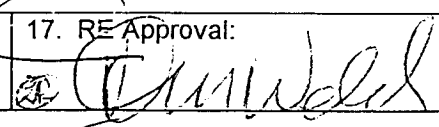
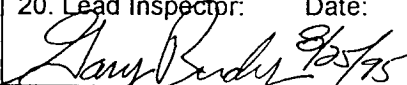
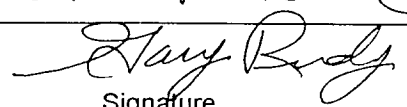
Call 9/6/95

- bc: M. Latch - Area 2
- A. Hodge - Area 2
- C. Dixon - B251
- A. Yahontov - EMC
- D. Compton - MTA
- G. Warren - MTA
- R. Trepp - PD/QA
- T. Welch - RE B252

Metropolitan Transportation Authority

May Affect ROD		
<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	<input type="checkbox"/> NA
		
Initials		Date

Nonconformance Report

1. Line: Contract No.: Year: NCR No.:		2. Contract Name:		3. Date:	
MRL B252 95 049		VERMONT SANTA MONICA STATION & CROSSOVER		8/4/95	
4. Location		5. NCR Type:		6. Contractor/Supplier:	
EXTERIOR WALLS GRID LINES 1-6.5		CONCRETE		KIEWIT	
7. Specification Section/Drawing No.:		8. Originator:		Phone No.:	
ACI 318		J. A. Vandrey		913-0095	
				Hold Tag No. N/A	
9. Contract requirement and nonconformance description:					
ACI 318 PARAGRAPH 7.3.2 - REBAR PARTIALLY EMBEDDED IN CONCRETE SHALL NOT BE BENT. FOURTEEN (14) #9 REBARS FOR THE INNER FACE OF THE EXTERIOR WALL, BETWEEN GRID LINES 1 & 6.5, ARE BENT 4" TO 31" OUT OF PLUMB. EXPOSED LENGTH IS 4'-6".					
10. Lead Inspector:		Date:		11. Reply requested from:	
Gary Rudy		8/4/95		SHERMAN LEE	
				12. Reply due date:	
				8/11/95	
13. Root cause of the problem and action(s) to prevent recurrence (completed by the contractor):					
DURING THE STRUT REMOVAL & FORMING OPERATION, OUR FIELD PERSONNEL ACCIDENTALLY BENT THE REBARS AS INDICATED ON ATTACHED SHEET 1/3. WE HAVE MODIFIED OUR OPERATION PROCEDURE WHICH WILL PREVENT THE PROBLEM FROM HAPPENING.					
14. Corrective action(s) (completed by the contractor):					
SEE ATTACHED SHEET 2/3.					
15. Prepared By:		Date:		16. Implementation Date:	
Sherman Lee		8-23-95		9-1-95	
				17. RE Approval:	
					
				Date: 8/24/95	
18. Disposition status: <input type="checkbox"/> Reject <input type="checkbox"/> Rework <input checked="" type="checkbox"/> Repair <input type="checkbox"/> Use-As-Is					
19. Engineer approval		Print Name:		Signature:	
(Repair and Use-As-Is):		John Sun		John Sun	
Repair by using the recommended procedure		8/25/95			
				Date: 8/25/95	
20. Lead Inspector: Date:					
Gary Rudy 8/25/95					
21. Verification that nonconforming condition has been corrected:					
EXISTING CONCRETE AROUND THE BENT BARS INSPECTED & FOUND TO BE ACCEPTABLE. BENT BARS STRAIGHTENED PER RECOMMENDED PROCEDURE - CONCRETE WAS THEN REINSPECTED & FOUND ACCEPTABLE - BARS ARE NOW ACCEPTABLE - (SEE PG. #3)					
22. Inspection:					
GARY RUDY				8/3/95	
Print Name		Signature		Date	

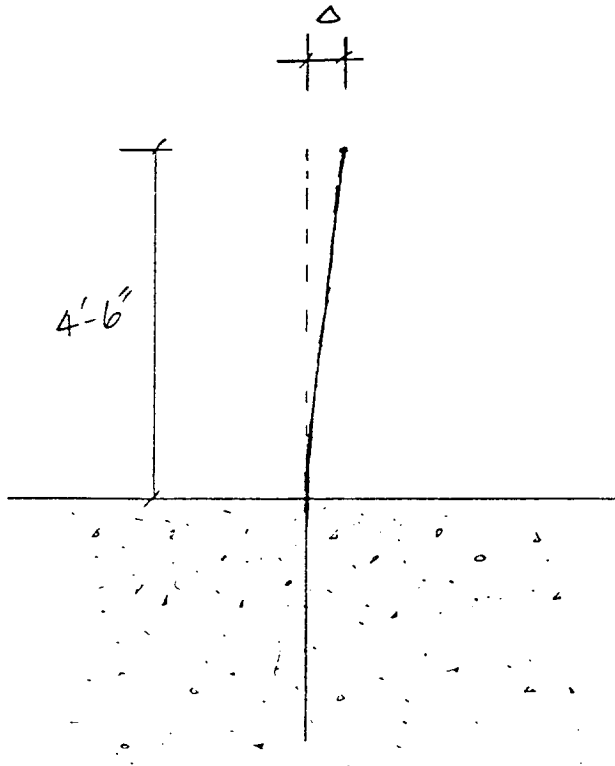
WORK SHEET

Project
Type of Work

BZ52 ^{Geo} 9/6/95

Estimator LEE
Date 8-23-95

Item No.
Sheet No.



LOCATION	Δ (IN.)	NO. OF BARS BEAT
# 1	4"	1
# 2	4"	1
# 3	4"	2
# 4	4"	1
# 5	4"	1
# 6	4"	2
# 7	4"	1
# 8	4"	1
# 10	4"	1
# 11	5"	2
# 12	5"	1
# 13	8"	1
# 14	31"	1

Project
Type of Work

B252

9/6/95

Estimator

LBE

Date

8-23-95

Item No.

Sheet No.

2/3

BY CONSIDERING THE LOADING & STRUCTURAL BEHAVIOR OF THE EXTERIOR WALL, THE INTERIOR FACE AT THE TOP OF INVERT SLAB WILL GENERALLY BE SUBJECTED TO COMPRESSION FORCE WHICH USUALLY MEANS THE REBAR IS NOT A CRITICAL STRUCTURE COMPONENT. PER ATTACHED ACI CODE, WE PROPOSE TO PROCEED WITH ONE OF THE FOLLOWING CORRECTIVE ACTIONS:

1. STRENGTHENING THE REBAR, THEN CONTINUING WITH VERTICAL REBAR SPLICE.
2. LEAVE BENT REBARS AS-IS AND PROCEED WITH OFFSET SPLICE.

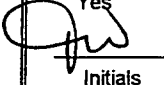
Recommended correction procedure for the bent rebars (Dowels):

1. Thoroughly inspect the existing concrete surface around the bent bars. If there is concrete crack or breakage, repair the defects to meet spec. requirements (Method such as epoxy pressure grout the concrete).
2. Straightening-up the rebars with bent off-set more than 5 inches from their original position. Proper method has to be used for straightening the rebar without damaging the concrete and rebar.
3. Process of straightening-up the bent bars should be gradual and follow all requirements of ACI-318-89 section 7.3.

Metropolitan Transportation Authority

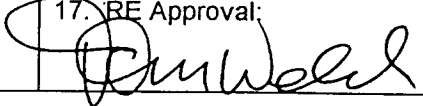

9/6/95

- bc: M. Latch - Area 2
- A. Hodge - Area 2
- C. Dixon - B251
- A. Yahontov - EMC
- D. Compton - MTA
- G. Warren - MTA
- R. Trepp - PD/QA
- T. Welch - RE B252

May Affect ROD		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Yes	No	NA
 Initials		8/27/95 Date

SEP

Nonconformance Report

1. Line: Contract No.: Year: NCR No.: MRL B252 95 051		2. Contract Name: VERMONT SANTA MONICA STATION & CROSSOVER		3. Date: 8/7/95	
4. Location S.O.S. # 4		5. NCR Type: CONCRETE		6. Contractor/Supplier: KIEWIT/CAT. PACIFIC	
7. Specification Section/Drawing No.: 03301			8. Originator: Gary Rudy 913-0095		Phone No.: Hold Tag No.
9. Contract requirement and nonconformance description: SPECIFICATION 03301 REQUIRES THAT MATERIALS USED IN CONCRETE, NOT BE USED FROM DIFFERENT &/OR UNAPPROVED SOURCES. APPROX. 160 C.Y. OF CONCRETE, (MIX # 4662CFWP4) HAS BEEN PLACED IN S.O.S. # 4. THE FINE AGGREGATE (SAND) IS FROM AN UNAPPROVED SOURCE.					
10. Lead Inspector: Gary Rudy		Date: 8/7/95		11. Reply requested from: S. LEE	
				12. Reply due date: 8/9/95	
13. Root cause of the problem and action(s) to prevent recurrence (completed by the contractor): BECAUSE OF THE STRIKE BY OPERATING ENGINEER, THE PLANT INSPECTOR FROM SMITH-EMERY CO. WAS NOT THERE TO VERIFY THE CONCRETE.					
14. Corrective action(s) (completed by the contractor): PER APPROVED SUBMITTAL # 3301-1.3-35.0, DATED 8-7-95, THE IN-PLACED APPROX. 160 CY CONCRETE IS ADEQUATE.					
15. Prepared By: Sherman Lee		Date: 8-21-95		16. Implementation Date: 8-7-95	
				17. RE Approval: 	
18. Disposition status: <input type="checkbox"/> Reject <input type="checkbox"/> Rework <input type="checkbox"/> Repair <input checked="" type="checkbox"/> Use-As-Is					
19. Engineer approval (Repair and Use-As-Is): SEE SUBMITTAL 03301-1.3-35.0 FOR SIGNATURE			Print Name: Signature: Date:		20. Lead Inspector: Date: Gary Rudy 8/27/95
21. Verification that nonconforming condition has been corrected: MIX DESIGN 4662CFWP4 HAS BEEN APPROVED (SEE SUBMITTAL 03301-1.3-35.00) COMPRESSIVE STRENGTH TESTS HAVE SHOWN THE IN PLACE CONCRETE TO BE 4578 PSI AT ELEVEN DAYS. ACCEPTABLE AS PLACED					
22. Inspection:		Gary Rudy Print Name		 Signature Date: 8/27/95	

Call 9/5/95

Metropolitan Transportation Authority

- bc: M. Latch - Area 2
- A. Hodge - Area 2
- C. Dixon - B251
- A. Yahontov - EMC
- D. Compton - MTA
- G. Warren - MTA
- R. Trepp - PD/QA
- T. Welch - RE B252

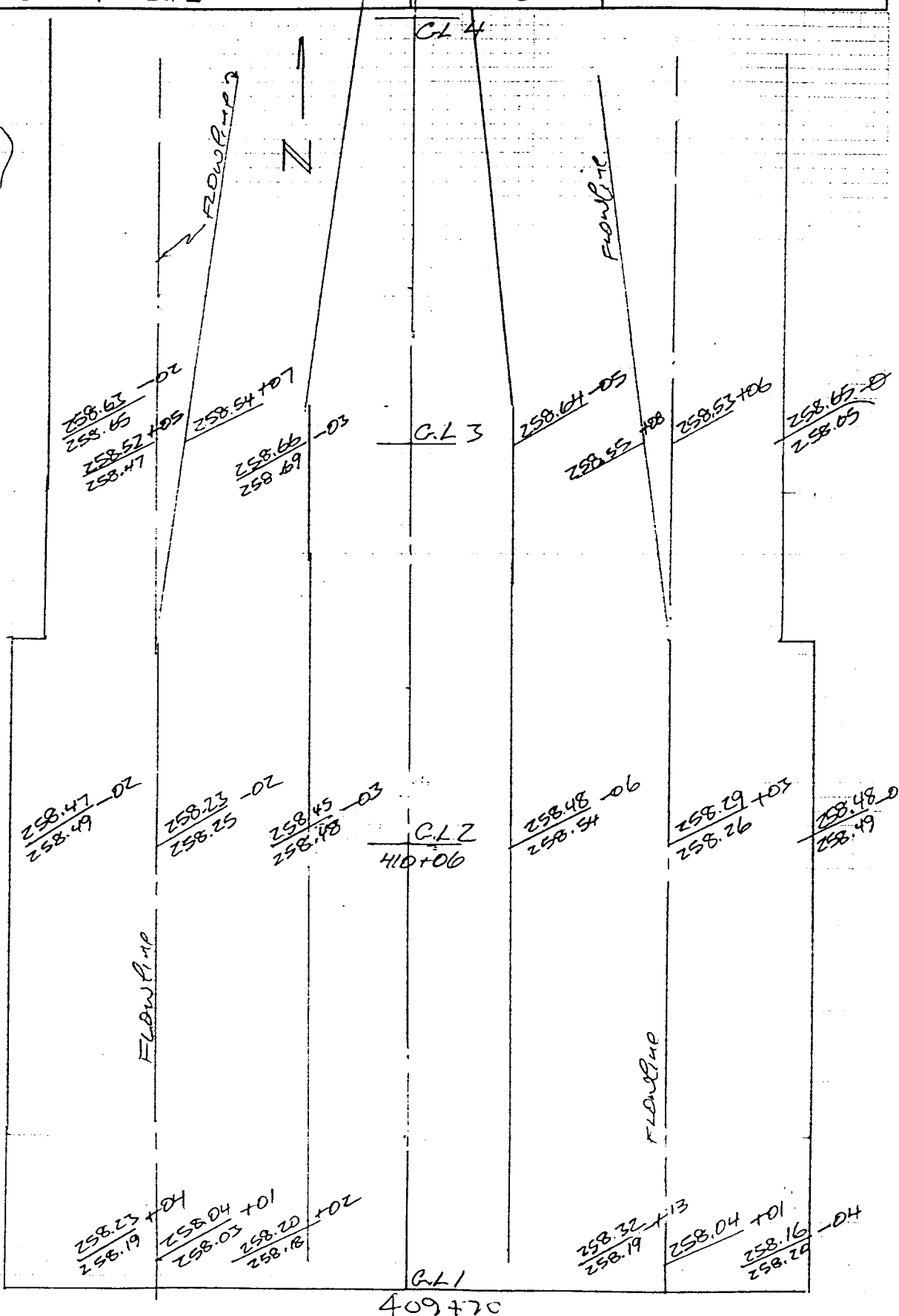
May Affect ROD		
<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> NA
_____ Initials		_____ Date

Nonconformance Report

1. Line: Contract No.: Year: NCR No.: <i>MRL B252 95 055</i>		2. Contract Name: <i>VERMONT SANTA MONICA STATION & CROSSOVER</i>		3. Date: <i>9/30/95</i>	
4. Location <i>INVERT SLAB 409+70 TO 411+39.67</i>		5. NCR Type: <i>CONCRETE</i>		6. Contractor/Supplier: <i>KIEWIT</i>	
7. Specification Section/Drawing No.: <i>03300</i>		8. Originator: <i>Gary Rudy</i>		Phone No.: <i>913-0095</i>	
9. Contract requirement and nonconformance description: <i>SPECIFICATION 03300, REQUIRES ELEVATION OF THE INVERT SLAB BE ± 1/4" FROM DESIGN. "AS BUILT" SURVEY OF THE FINISHED SLAB, FROM 409+70 TO 411+39.67 INDICATES THAT THE ELEVATION VARIES FROM + 1 1/2" TO - 1/4" FROM THE DESIGN ELEVATION. (SEE ATTACHMENT #1 & 2)</i>					
10. Lead Inspector: <i>Gary Rudy</i>		Date: <i>9/30/95</i>		11. Reply requested from: <i>S. LEE</i>	
12. Reply due date: <i>9/8/95</i>					
Root cause of the problem and action(s) to prevent recurrence (completed by the contractor):					
14. Corrective action(s) (completed by the contractor):					
15. Prepared By:		Date:		16. Implementation Date:	
				17. RE Approval:	
				Date:	
18. Disposition status: <input type="checkbox"/> Reject <input type="checkbox"/> Rework <input type="checkbox"/> Repair <input type="checkbox"/> Use-As-Is					
19. Engineer approval (Repair and Use-As-Is):		Print Name: Signature:		Date:	
				20. Lead Inspector: Date:	
21. Verification that nonconforming condition has been corrected:					
22. Inspection:					
		Print Name		Signature	
				Date	

SURVEY NOTES, 1/10" GRID			FILE 8-252 SANTA Monica		
PARTY CHIEF K. D. Dineby	RECORDER	INSTRUMENT MAN	WEATHER	DATE OF SURVEY 7-27-95	PAGE OF 1 2
SURVEY DESCRIPTION Base Slats As Built			COMPUTED BY K.D	CHECKED BY K.D	

As Built DESIGN TYP



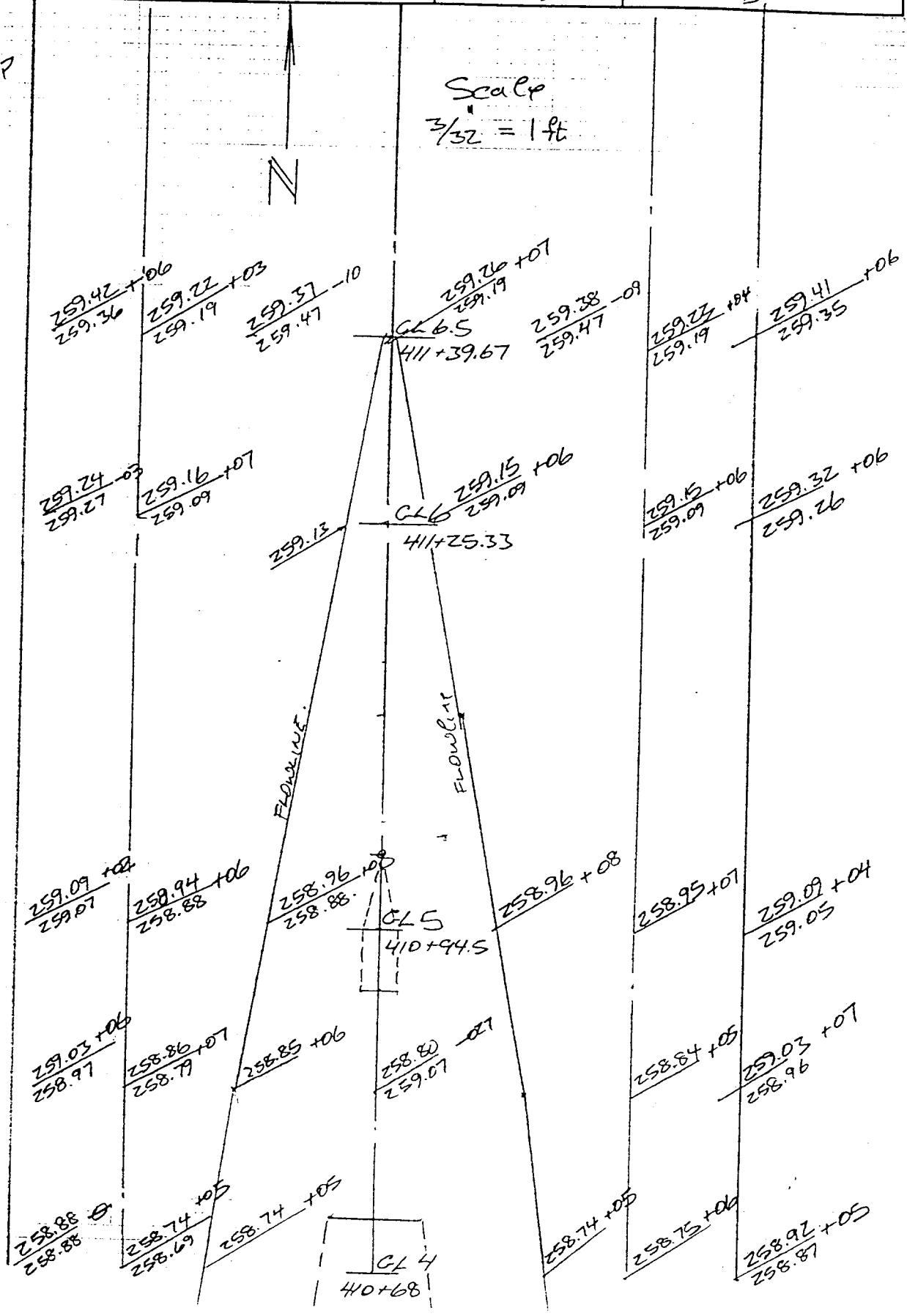
SURVEY NOTES, 1/10" GRID

FILE		B-252 Santa Monica	
PARTY CHIEF	RECORDER	INSTRUMENT MAN	WEATHER
K. D. Shetty		R. Gonzalez	
DATE OF SURVEY		PAGE OF	
7-27-95		2 2	
COMPUTED BY		CHECKED BY	
K.D.		K.D.	

SURVEY DESCRIPTION
 Base Slabs As BUILT

AS BUILT
 DESIGN TYP

Scale
 $\frac{3}{32} = 1 \text{ ft}$



See 9/6/95

May Affect ROD		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Yes	No	NA
_____		_____
Initials		Date

Nonconformance Report

1. Line: MRL	Contract No.: C311	Year: 1995	NCR No.: 008	2. Contract Name: Tunnel Line Section, Sta 630+00 to Universal City	3. Date: 08-30-95
4. Location Cross-over excavation pile PS2		5. NCR Type: Soldier Pile		6. Contractor/Supplier: Traylor Bros./ Frontier Kemper	
7. Specification Section/Drawing No.: Section 02160, 05120			8. Originator: G. J. E. Kramer	Phone No.: 818-763-3015	Hold Tag No.
9. Contract requirement and nonconformance description: On Monday, August 7, 1995 when the fabricated steel section for soldier pile S1 of the cross-over excavation was picked up for placement, it was severely damaged in bending at approximately mid-length due to an improperly placed sling. The steel section designated for pile S2 was then used in pile location S1 with an unconfirmed toe penetration depth. A repair of the bent pile was effected by the contractor without approved working drawings. This steel section was subsequently installed as soldier pile S2 on Monday, August 14, 1995 with the knowledge of the Resident Engineer and the inspector. This work is in non-conformance with the Contract C0311 Specification Sections 03100-3.7D, 02160-1.2B, 02160-1.3A.1 and 05120-1.3B.					
10. Lead Inspector: E. Kramer Asst. R.E.		Date: 08-30-95	11. Reply requested from: TBFK/M. Roach		12. Reply due date: 9/15/95
13. Root cause of the problem and action(s) to prevent recurrence (completed by the contractor):					
14. Corrective action(s) (completed by the contractor):					
15. Prepared By:		Date:	16. Implementation Date:	17. RE Approval:	Date:
18. Disposition status: <input type="checkbox"/> Reject <input type="checkbox"/> Rework <input type="checkbox"/> Repair <input type="checkbox"/> Use-As-Is					
19. Engineer approval (Repair and Use-As-Is):			Print Name:	Signature:	Date:
			20. Lead Inspector: _____ Date: _____		
21. Verification that nonconforming condition has been corrected:					
22. Inspection:					
		Print Name		Signature	
				Date	

See 9/6/95

May Affect ROD		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Yes	No	NA
_____ Initials		_____ Date

Nonconformance Report

1. Line: MRL	Contract No.: C311	Year: 1995	NCR No.: 009	2. Contract Name: Tunnel Line Section, Sta 630+00 to Universal City	3. Date: 08-30-95
4. Location Cross-over excavation pile PW4 level A		5. NCR Type: Tie-back		6. Contractor/Supplier: Traylor Bros./ Frontier Kemper	
7. Specification Section/Drawing No.: Section 02160, 02162			8. Originator: G. J. E. Kramer	Phone No.: 818-763-3015	Hold Tag No.
9. Contract requirement and nonconformance description: On Monday, August 21, 1995 the tie-back at level A for soldier pile PW4 of the cross-over excavation support system was installed by a new crew from Malcolm Drilling (sub-contractor to TBFK). The hole was over-drilled with 2 extra lengths of drill casing. This resulted in an approximate excess length of 10'. Unaware of the excess length, the crew inserted the tie-back cable bundle into the hole resulting in the bundle being too short to extend outside of the hole. Efforts to retrieve the bundle from within the hole were unsuccessful, the cable bundle was abandoned and the hole grouted. Subsequently, a new hole was drilled to the correct length directly beneath the failed hole and at a 5 degree steeper angle from the horizontal than called for in the original design submission. To date, this hole has been grouted and post-grouted successfully. The installation of the work deviates from the approved submittals and has now been installed in a zone of potentially weakened or disturbed soil created by the previously failed tie-back installation and possible physical interference or load influence with future lower anchors. This work is non-conformance with the Contract C-0311 Specification Sections 03100-1.2, 0216203.1A and TBFK's drawings submitted under Section 02160-1.3A.					
Lead Inspector: G. J. E. Kramer Asst. R.E.		Date: <i>[Signature]</i> 08-30-95	11. Reply requested from: TBFK/M. Roach		12. Reply due date: 9/15/95
13. Root cause of the problem and action(s) to prevent recurrence (completed by the contractor):					
14. Corrective action(s) (completed by the contractor):					
15. Prepared By:		Date:	16. Implementation Date:	17. RE Approval:	Date:
18. Disposition status: <input type="checkbox"/> Reject <input type="checkbox"/> Rework <input type="checkbox"/> Repair <input type="checkbox"/> Use-As-Is					
19. Engineer approval (Repair and Use-As-Is):			Print Name:	Signature:	Date:
21. Verification that nonconforming condition has been corrected:			20. Lead Inspector: Date:		
22. Inspection:					
		Print Name		Signature	
				Date	

Case
7/6/95

May Affect ROD		
<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	<input type="checkbox"/> NA
_____ Initials		_____ Date

Nonconformance Report

1. Line: MRL	Contract No.: C311	Year: 1995	NCR No.: 010	2. Contract Name: Tunnel Line Section, Sta 630+00 to Universal City	3. Date: 08-30-95
4. Location Cross-over excavation piles PW53 & PE53		5. NCR Type: Soldier Pile/Tieback		6. Contractor/Supplier: Traylor Bros./ Frontier Kemper	
7. Specification Section/Drawing No.: Section 02160, 05120			8. Originator: G. J. E. Kramer	Phone No.: 818-763-3015	Hold Tag No.
9. Contract requirement and nonconformance description: On Monday, August 28, 1995 it was observed that the tie-backs at levels A, B, and C for soldier piles PW53 and PE53 of the cross-over excavation support system have all been installed on the same side of the web of the steel pile. This was discovered upon observations of a uniform twist of the pile and the flange of the pile drawing away from lagging on the north side. The tie-backs have been successfully installed and tested according to the contract specifications. TBFK submittal drawings ES-12 Rev.2 shows that subsequent tie-back levels should be installed on alternate sides of the web. Apparently, the tie-backs have been installed on the same side of the web due to insufficient clearance on the north side of the pile for the drill rig boom. The installation of the work deviates from the approved submittals and is in non-conformance with the Contract C-0311 Specification Sections 03100-1.2B and TBFK's drawings submitted under Section 02160-1.3A.					
10. Lead Inspector: G. J. E. Kramer Asst. R.E. <i>[Signature]</i>		Date: 08-30-95	11. Reply requested from: TBFK/M. Roach		12. Reply due date: 9/15/95
Root cause of the problem and action(s) to prevent recurrence (completed by the contractor):					
14. Corrective action(s) (completed by the contractor):					
15. Prepared By:		Date:	16. Implementation Date:	17. RE Approval: Date:	
18. Disposition status: <input type="checkbox"/> Reject <input type="checkbox"/> Rework <input type="checkbox"/> Repair <input type="checkbox"/> Use-As-Is					
19. Engineer approval (Repair and Use-As-Is):			Print Name:	Signature:	Date:
			20. Lead Inspector: Date:		
21. Verification that nonconforming condition has been corrected:					
22. Inspection:					
			Print Name		Date
			Signature		Date